



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

**Anthony Lee Washington III**  
**2025.01.07 15:29:57-06'00'**

PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE ELECTRONIC DOCUMENTS.

HDR ENGINEERING, INC.  
120 BRENTWOOD COMMONS WAY  
SUITE 525  
BRENTWOOD, TN 37027  
ANTHONY L. WASHINGTON III, P.E. NO. 119749

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEETS .....	ROADWAY-SIGN1
TITLE SHEET .....	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS AND STANDARD TRAFFIC OPERATIONS DRAWINGS .....	1A
PROJECT COMMITMENTS .....	1B
ESTIMATED ROADWAY QUANTITIES .....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....	2B, 2B1, 2B2
GENERAL NOTES .....	2C
SPECIAL NOTES .....	2D
ENVIRONMENTAL NOTES .....	2E, 2E1
TABULATED QUANTITIES.....	2F
UTILITY NOTES AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	4

YEAR	PROJECT NO.	SHEET NO.
2025	NH-I-240-1(299)	ROADWAY-SIGN 1

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SIGNATURE  
SHEET

Index Of Sheets  
SEE SHEET NO. 1A

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF ENGINEERING

SHELBY COUNTY

I-240  
FROM: L.M. 15.83 (NEAR POPLAR AVENUE) (SR-57)  
TO: L.M. 19.98 (NEAR SUMMER AVENUE) (SR-1)

RESURFACE  
MILL, 411D, 411E (SHOULDERS) AND PAVEMENT MARKINGS

STATE HIGHWAY NO. N/A F.A.H.S. NO. I-240

79I240-F8-007  
END PROJECT NO. NH-I-240-1(299) RESURFACE  
L.M. 19.98 (NEAR SUMMER AVENUE) (SR-1)

BRIDGE-DECK-REPAIR PROJECT NO. 79I240-M3-009  
I-240 - @ L.M. 19.18

79I240-F8-007  
BEGIN PROJECT NO. NH-I-240-1(299) RESURFACE  
L.M. 15.83 (NEAR POPLAR AVENUE) (SR-57)

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

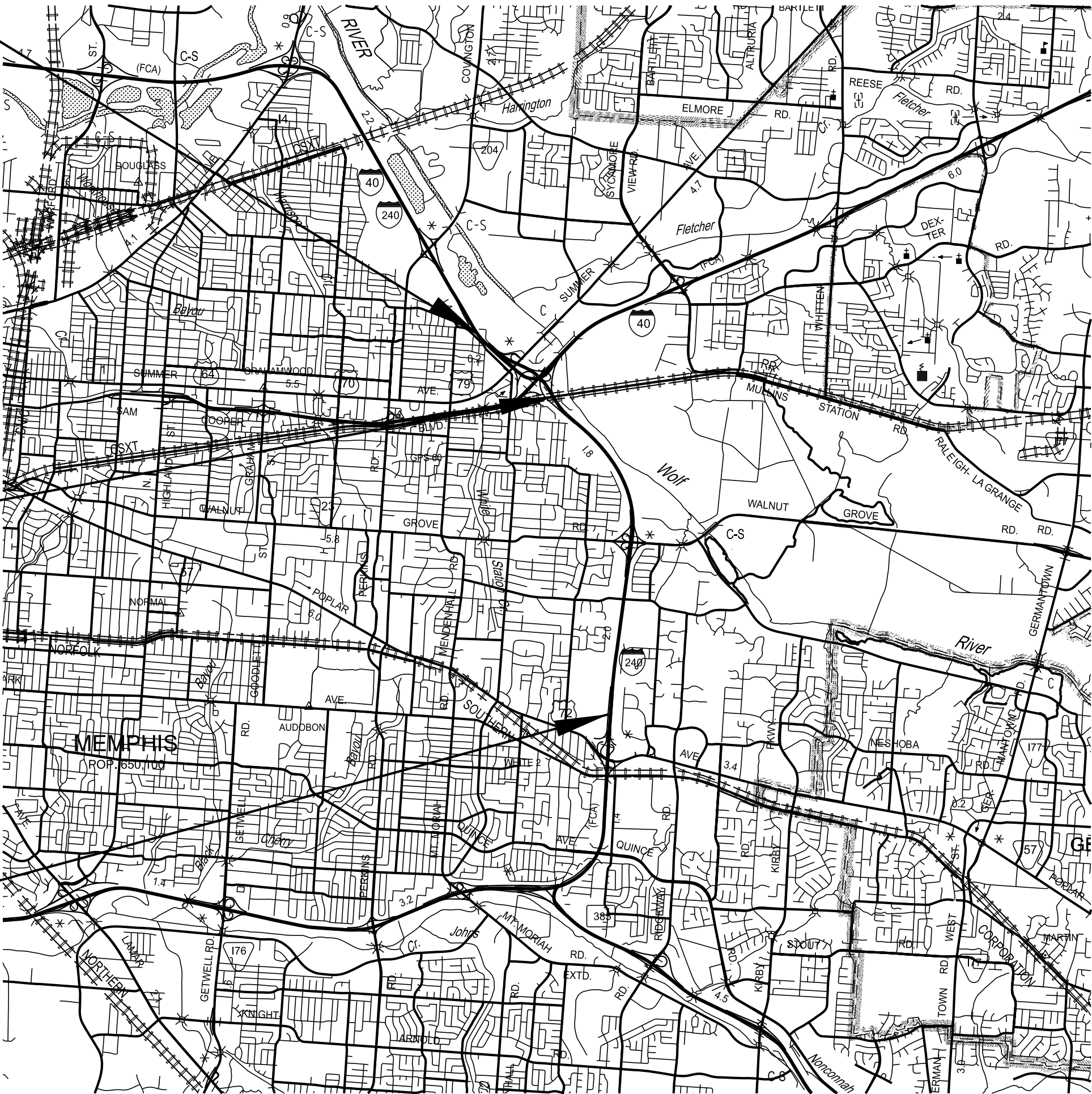
TDOT PROJECT MANAGER : LYNN EVANS, P.E., REG. 4

DESIGNED BY : HDR ENGINEERING, INC.

DESIGNER : ANTHONY L. WASHINGTON, III, P.E. CHECKED BY DAVID HORNE, P.E.

P.E. NO. 98043-4175-04

PIN NO. 132479.00

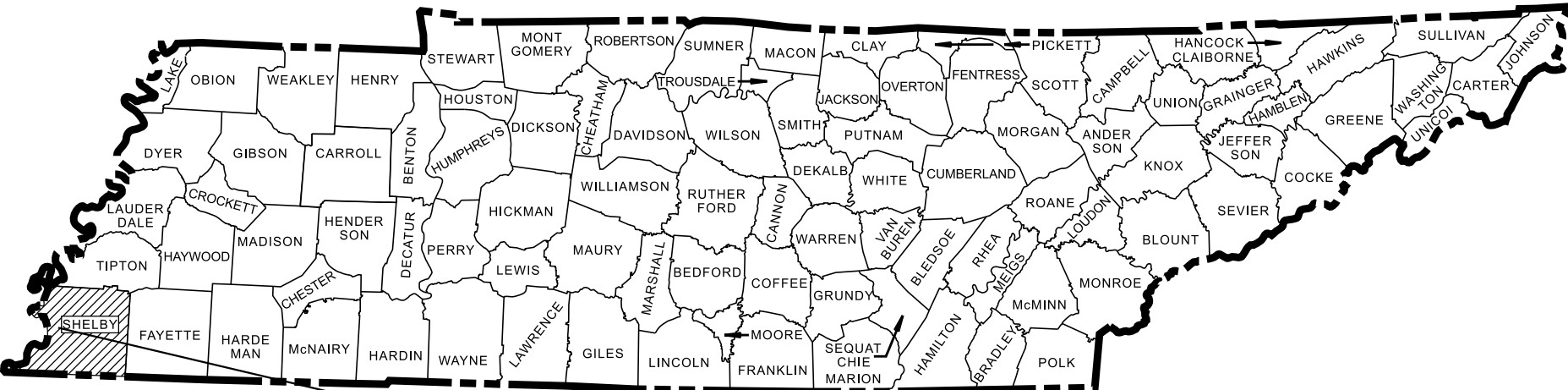


SCALE: 1" = 5280'

PROJECT LENGTH 4.15 MILES  
TOTAL LANE MILES RESURFACED 33.53 MILES

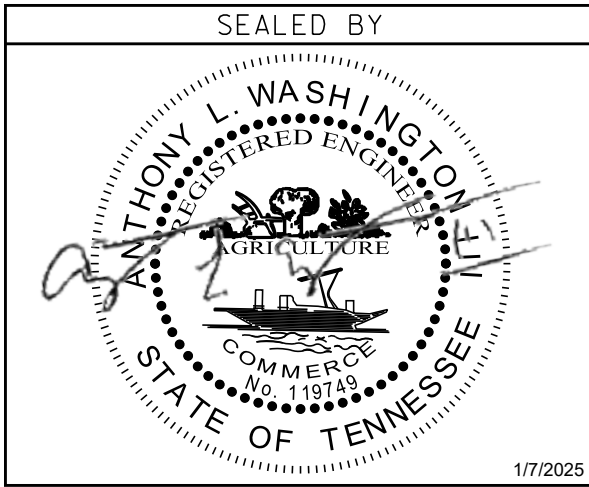
DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES X	NO _

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.	NH-I-240-1(299)	
STATE PROJ. NO.	79I240-F8-007	
STATE PROJ. NO.	79I240-M3-009	



PROJECT LOCATION  
BRIDGE ID. # 79I02400115 79I02400117 79I02400119  
79I02400120 79I02400136B 79I00400167  
79I00400102 79I00400101 79I00400168B  
79I00400099 79I00400097 79I00400098  
79I00400095 79I00400096

NO EXCLUSIONS



APPROVED: WILL REID, CHIEF ENGINEER

DATE:

APPROVED: HOWARD H. ELEY, COMMISSIONER

TRAFFIC COUNTER,  
EMBEDDED DETECTION LOOPS &  
AUTOMATIC TRAFFIC READERS

STATION LOCATION	LOG MILE
A.T.R.31 # 231 EB	17.040
A.T.R.31 #431 WB	17.040
TC STATION 189	18.901

TRAFFIC DATA	
ADT (2025)	194,438
POSTED SPEED LIMITS	
L.M. 15.83 TO L.M. 19.98	55 MPH

U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: DIVISION ADMINISTRATOR DATE

ROADWAY INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEETS.....	ROADWAY-SIGN1
TITLE SHEET .....	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, AND STANDARD TRAFFIC OPERATIONS DRAWINGS.....	1A
PROJECT COMMITMENTS .....	1B
ESTIMATED ROADWAY QUANTITIES.....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE.....	2B, 2B1, 2B2
GENERAL NOTES .....	2C
SPECIAL NOTES .....	2D
ENVIRONMENTAL NOTES.....	2E, 2E1
TABULATED QUANTITIES.....	2F
UTILITY NOTES AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	4
TRAFFIC DETECTION LOOPS DETAIL.....	5
BRIDGE PLANS .....	B-1

NOTE: THE ALPHABETICAL LETTERS “I”, “O” & “Q” ARE NOT USED IN THE NUMBERING OF SHEETS.

NO UTILITY SHEETS INCLUDED IN THIS SET OF PLANS.

STANDARD ROADWAY DRAWINGS

DWG.	REV.	DESCRIPTION
STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS		
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
RD-L-3	03-01-23	STANDARD LEGEND FOR SIGNALIZATION AND LIGHTING
DESIGN - TRAFFIC CONTROL		
T-M-3	07-07-23	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS
T-M-5	03-01-23	MARKING DETAIL FOR FREEWAYS
T-M-6	03-01-23	MARKING DETAIL FOR EXPRESSWAY AND FREEWAY INTERCHANGES
T-M-7	06-28-19	GORE MARKING DETAILS FOR EXPRESSWAY & FREEWAY INTERCHANGES
T-M-8	06-28-19	MARKING DETAILS FOR EXPRESSWAYS & FREEWAYS
T-M-9	05-01-23	PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-9A		PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-9B		PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-15	06-28-19	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR INTERSTATE AND ACCESS CONTROLLED ROUTES
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-04-21	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-12	03-04-21	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
T-WZ-14	03-04-21	TWO-OUTSIDE LANE CLOSURE FOR INTERSTATES AND EXPRESSWAYS
T-WZ-18	07-07-23	SHOULDER CLOSURE DETAIL FOR FREEWAYS AND DIVIDED HIGHWAYS
T-WZ-60		FREEWAY RESURFACING SIGNING LAYOUT
T-WZ-63	01-09-24	WORK ZONE IN THE VICINITY OF AN ENTRANCE RAMP
T-WZ-64		WORK ZONE IN THE VICINITY OF AN EXIT
T-WZ-FAB1		FLASHING YELLOW ARROW BOARD


STANDARD TRAFFIC OPERATIONS DRAWINGS

DWG.	REV.	DESCRIPTION
T-SG-2	06-27-16	LOOP LEAD-INS, CONDUIT AND PULL BOXES
T-SG-3	07-11-17	STANDARD NOTES AND DETAILS OF INDUCTIVE LOOPS
T-SG-3A	06-27-16	ALTERNATE DETECTION DETAILS

SIGNALS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-240-1(299)	1A

SEALED BY



1/7/2025

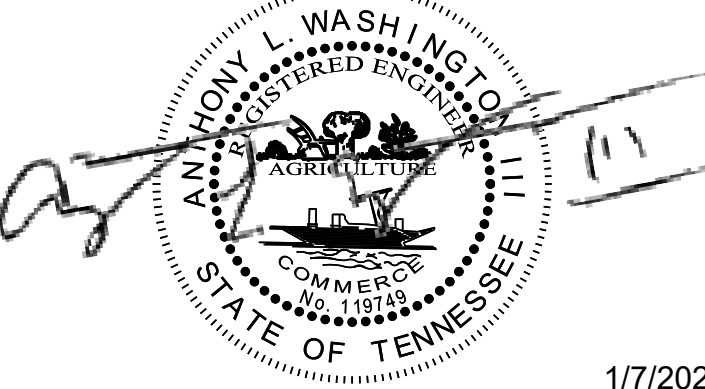
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX,  
STANDARD ROADWAY  
DRAWINGS, AND  
STANDARD TRAFFIC  
OPERATIONS DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-240-1(299)	1B

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	AN ASBESTOS CONTAINING MATERIAL (ACM) SURVEY WAS COMPLETED ON BRIDGE NO. 79I02400119 I-240 EB OVER PEDESTRIAN FACILITY LM 19.18 (79-I0240-19.18). NO ASBESTOS WAS DETECTED. PLEASE SEE THE REPORT FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHA-BILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03).	BRIDGE NO. 79I02400119 I-240 EB OVER PEDESTRIAN FACILITY LM 19.18 (79-I0240-19.18)

SEALED BY



1/7/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PROJECT  
COMMITMENTS



ESTIMATED ROADWAY QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY 791240-F8-007
202-03.01	REMOVAL OF ASPHALT PAVEMENT	S.Y.	4040
203-06	WATER	M.G.	7
(1) 208-01.05	BROOMING & DEGRASSING SHOULDERS	L.M.	17
(1) 303-02	MINERAL AGGREGATE, TYPE B BASE, GRADING (C OR D)	TON	472
(2) 307-03.08	ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING B-M2	TON	914
(3)(4) 403-02.01	TRACKLESS TACK COAT	TON	230
411-01.07	ACS MIX (PG64-22) GRADING E SHOULDER	TON	7144
(5)(6) 411-01.21	LONGITUDINAL JOINT SEALANT	L.M.	43
(4)(7) 411-03.10	ACS MIX(PG76-22) GRADING D	TON	21920
411-12.01	SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M.	14
(4)(8) 415-01.01	COLD PLANING BITUMINOUS PAVEMENT	TON	28713
(9) 712-01	TRAFFIC CONTROL	LS	1
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	616
712-05.01	WARNING LIGHTS (TYPE A)	EACH	54
712-05.03	WARNING LIGHTS (TYPE C)	EACH	562
(10) 712-06	SIGNS (CONSTRUCTION)	S.F.	1911
(11) 712-06.16	SIGNS (CONSTRUCTION)(REDUCED SPEED WARNING)	EACH	10
712-08.03	ARROW BOARD (TYPE C)	EACH	2
712-08.08	SPEED FEEDBACK SIGN ASSEMBLY	EACH	2
712-08.09	DIGITAL SPEED LIMIT SIGN ASSEMBLY	EACH	9
712-08.12	QUEUE PROTECTION TRUCK	DAY	120
713-16.01	CHANGEABLE MESSAGE SIGN UNIT	EACH	4
716-01.22	SNOWPLOWABLE RAISED PAVMENT MARKERS (MONO-DIR)(1 COLOR)	EACH	739
(4) 716-01.23	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	4010
(4)(12) 716-01.30	REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	4749
(13)(14) 716-02.04	PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	1068
(13)(14) 716-02.07	PLASTIC PAVEMENT MARKING (24" BARRIER LINE)	L.F.	4356
(4)(13)(14) 716-02.23	PLASTIC PAVEMENT MARKING (12IN BARRIER LINE)	L.F.	6257
(13)(14) 716-03.09	PLASTIC WORD PAVEMENT MARKING (EAST)	EACH	4
(13)(14) 716-03.10	PLASTIC WORD PAVEMENT MARKING (WEST)	EACH	4
(13)(14) 716-04.04	PLASTIC PAVEMENT MARKING (TRANSVERSE SHOULDER)	L.F.	193
(13)(14) 716-04.07	PLASTIC PAVEMENT MARKING (EXIT ONLY ARROW)	EACH	11
(15) 716-05.20	PAINTED PAVEMENT MARKING (6" LINE)	L.M.	58.9
(14) 716-10.50	PREFORMED PLASTIC PAVEMENT MARKING (INTERSTATE SHIELD)	EACH	8
(4)(14) 716-12.02	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	58.9
(4)(14) 716-12.03	ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE)	L.F.	12686
(4)(14) 716-12.05	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN DOTTED LINE)	L.F.	10641
(13)(14) 716-12.09	ENHANCED FLAT LINE THERMO (12IN LINE)	L.F.	249
(14) 716-12.10	ENHANCED FLAT LINE THERMO (12IN DOTTED)	L.F.	10560
717-01	MOBILIZATION	LS	1
730-03.25	INSTALL PULL BOX (29" X 21")	EACH	2
730-05.01	ELECTRICAL SERVICE CONNECTION	EACH	2
730-12.02	CONDUIT 2" DIAMETER (PVC)	L.F.	50
730-14.02	SAW SLOT	L.F.	840
730-14.03	LOOP WIRE	L.F.	3740
(16) 730-15.07	CABINET	EACH	2


FOOTNOTES	
(1)	TO BE USED AS DIRECTED BY THE ENGINEER.
(2)	FOR REPLACEMENT OF QUANTITY REMOVED UNDER ITEM 202-03.01.
(3)	INCLUDES 40 TONS FOR RAMP REPAIR.
(4)	SEE RAMP, ACCELERATION LANE, AND DECELERATION LANE TABULATION ON SHEET 2F FOR QUANTITIES ASSOCIATED WITH RAMPS.
(5)	USE CRAFCO PAVEMENT JOINT ADHESIVE #34524. PAVON JOINT ADHESIVE BY PAVON CORPORATION OR DENSO TAPE BY DENSO.
(6)	TO BE USED FOR SEALING OF ALL SURFACE LAYER CONSTRUCTION JOINTS ALONG THE TRAVEL LANES AND SHOULDERS AS DIRECTED BY THE ENGINEER
(7)	INCLUDES 6,282 TONS FOR RAMP REPAIR.
(8)	INCLUDES 7,077 TONS FOR SHOULDERS, 5,908 TONS FOR RAMP REPAIRS, AND 240 TONS FOR VARIOUS LOCATIONS THROUGHOUT THE PROJECT LIMITS.
(9)	THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
(10)	THE CONTRACTOR IS RESPONSIBLE FOR THE STAKING OF CONSTRUCTION SIGNS. IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING APPROVED BY THE ENGINEER, TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO 712-06 SIGNS (CONSTRUCTION).
(11)	ITEM TO BE USED ONLY WHEN A REDUCED SPEED LIMIT IS ESTABLISHED WITHIN THE PROJECT CONSTRUCTION WORK ZONE LIMITS. ITEM INCLUDES SIGN FACE, SUPPORTS, AND TWO TYPE "B" FLASHERS PER THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TURNING ON THE TYPE "B" FLASHERS WHEN WORKERS ARE IN THE CONSTRUCTION WORK ZONE AND TURNING THEM OFF WHEN WORKERS ARE NO LONGER IN THE CONSTRUCTION WORK ZONE.
(12)	FOR REMOVAL OF EXISTING PAVEMENT MARKERS.
(13)	THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
(14)	ITEM TO BE USED FOR FINAL PAVEMENT MARKING ONLY.
(15)	ITEM TO BE USED FOR TEMPORARY PAVEMENT MARKING ONLY.
(16)	MINIMUM SIZE OF CABINET SHALL BE 41" TALL, 25" WIDE AND 18" DEEP. ITEM INCLUDES COMPLETE INSTALLATION PER SPECIAL TRAFFIC COUNTER SPECIFICATION NOTES CONCERNING THE COUNTER CABINET. ITEM INCLUDES THE 4" GALVANIZED, RIGID CONDUIT POLE THAT THE CABINET IS TO BE MOUNTED ONTO.

THERE IS NO GUARDRAIL WORK ON THIS PROJECT

THERE ARE NO UTILITY ADJUSTMENTS ON THIS PROJECT.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-240-1(299)	2

SEALED BY

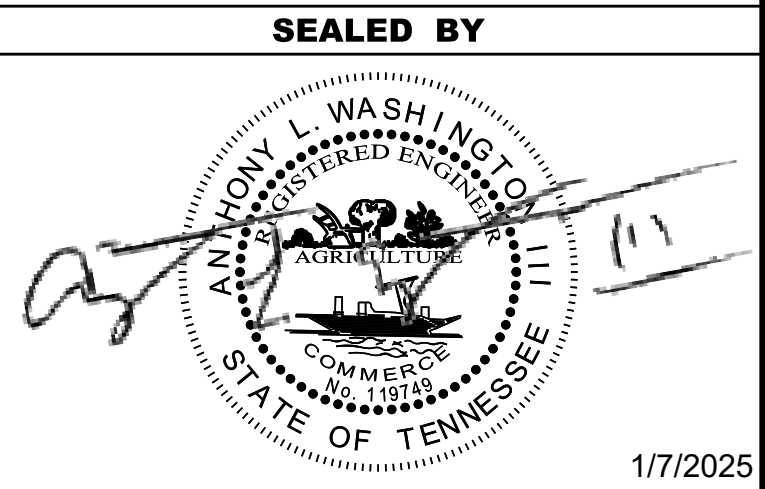
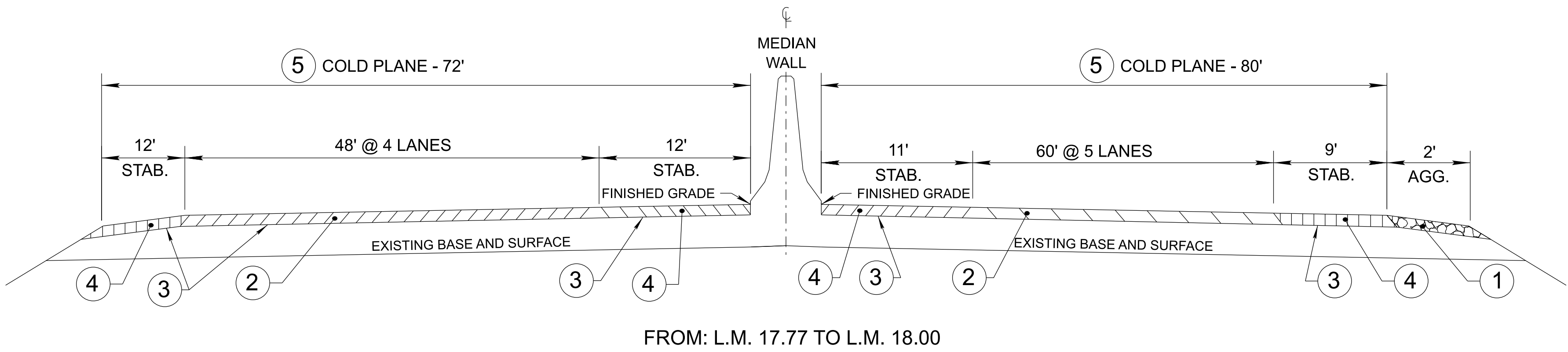
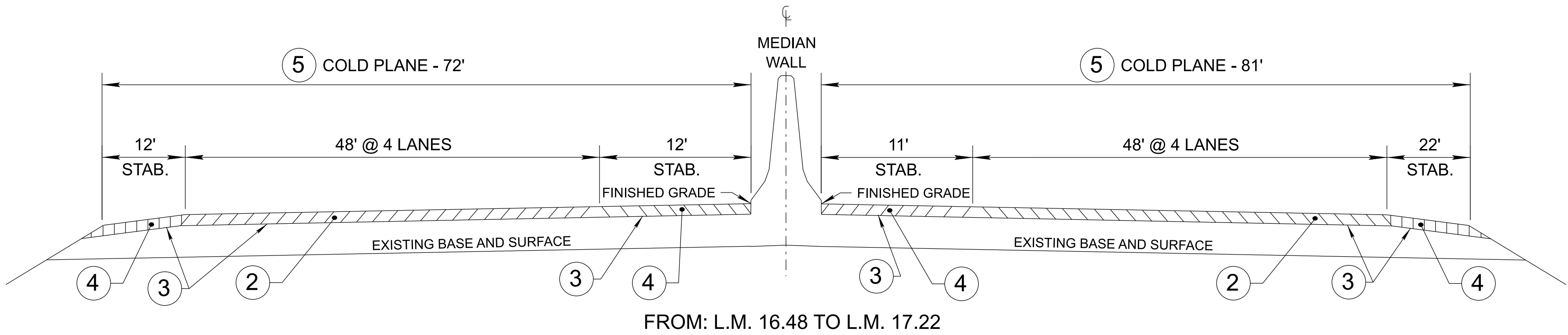
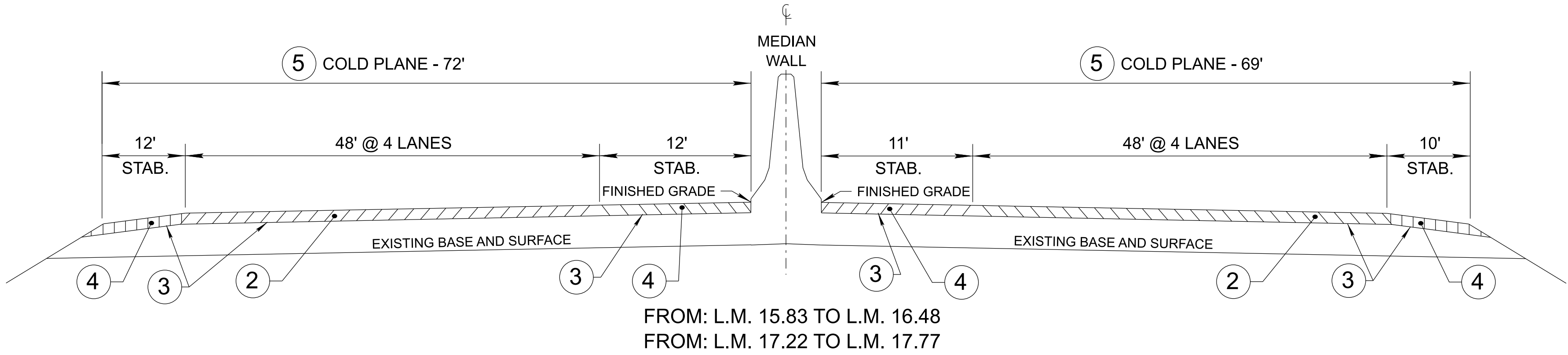


1/7/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ESTIMATED  
ROADWAY  
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-240-1(299)	2B

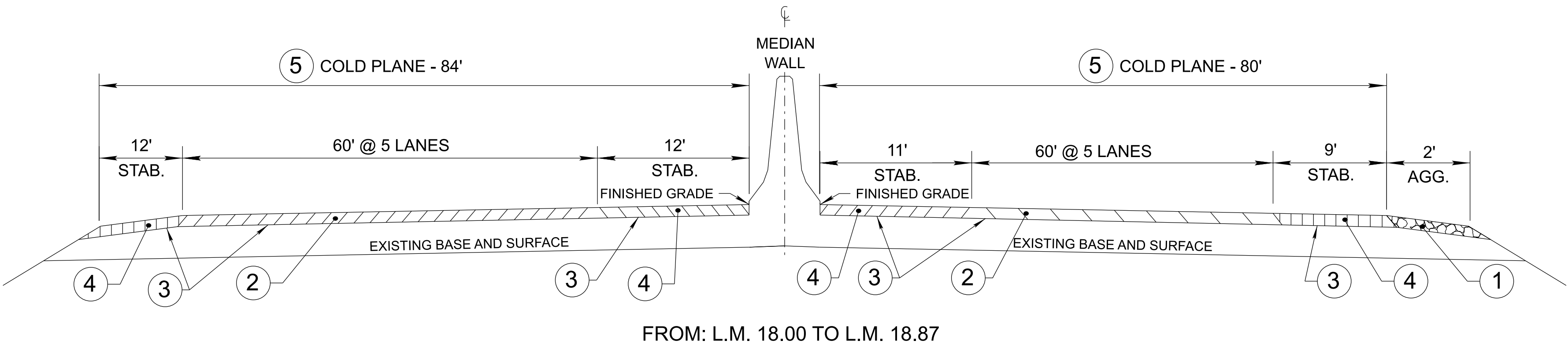


NOT TO SCALE

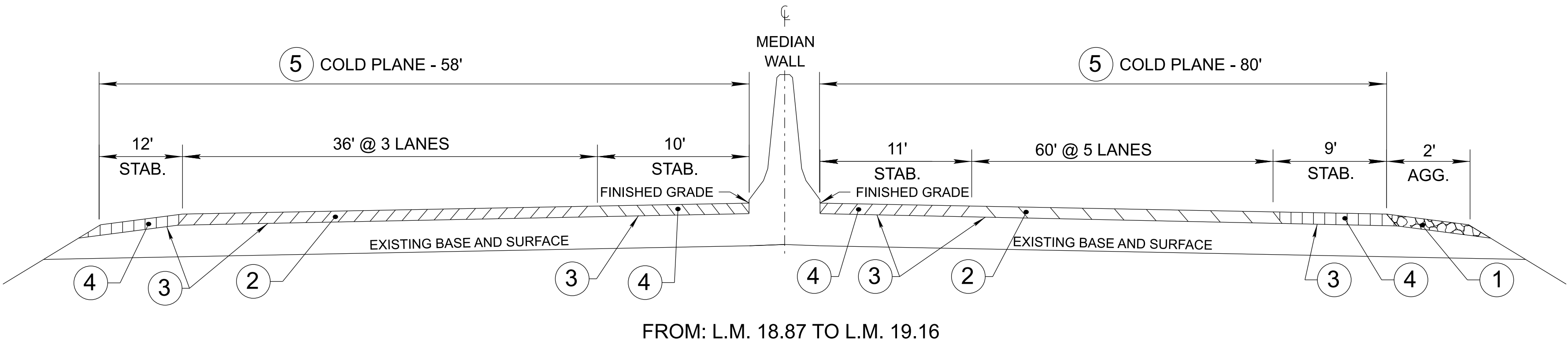
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TYPICAL  
SECTIONS

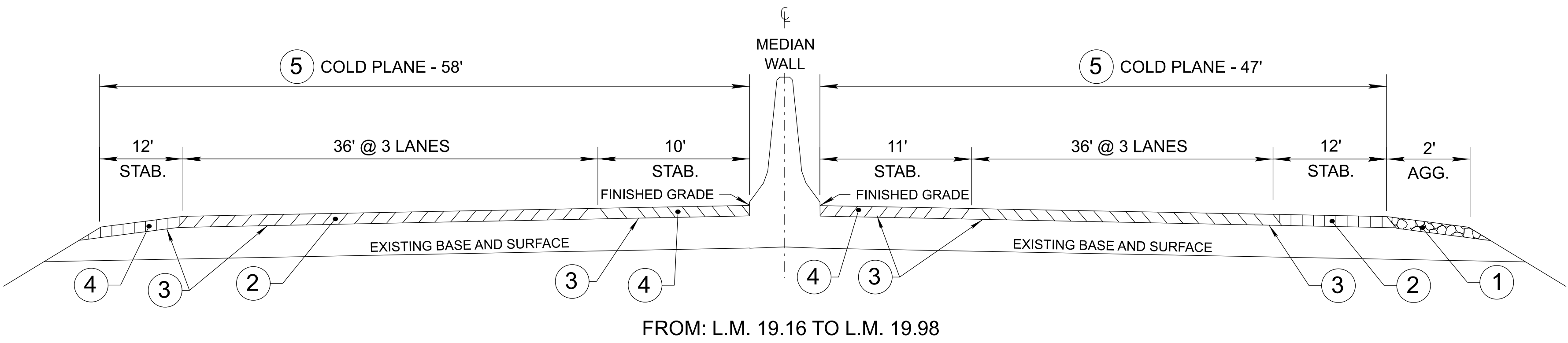
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-240-1(299)	2B1



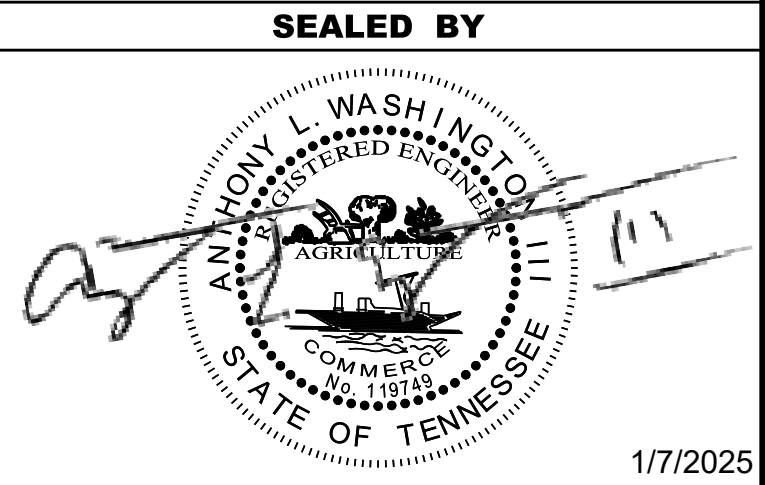
FROM: L.M. 18.00 TO L.M. 18.87



FROM: L.M. 18.87 TO L.M. 19.16



FROM: L.M. 19.16 TO L.M. 19.98



1/7/2025

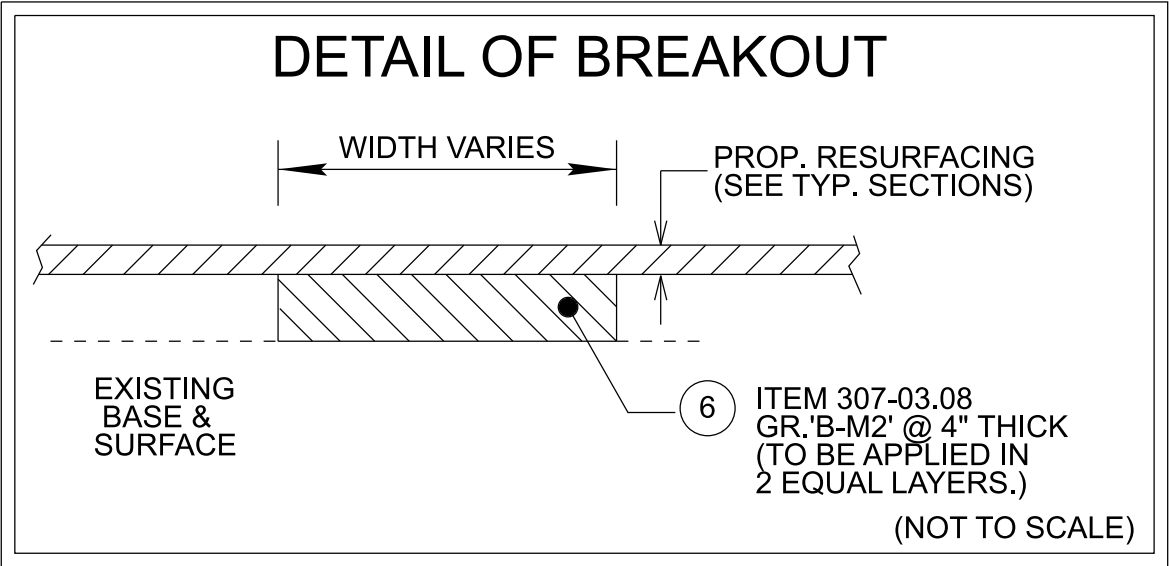
NOT TO SCALE

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TYPICAL  
SECTIONS

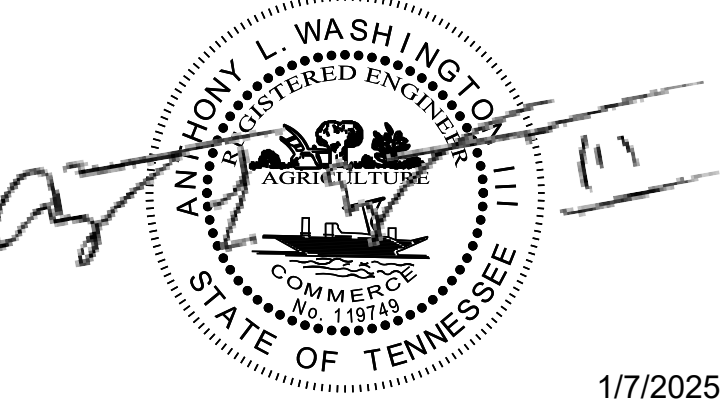
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-240-1(299)	2B2

PROPOSED PAVEMENT SCHEDULE	
① MINERAL AGGREGATE BASE @ 2.00"± THICK FOR SHOULDERS ITEM 303-02 MINERAL AGGREGATE, TYPE "B" BASE, GRADING "C OR D"	② ASPHALTIC CONCRETE SURFACE (HM) @ 1.25"± THICK (APPROX. 132.5 LBS./S.Y.) ITEM 411-03.10 ACS MIX (PG76-22) GRADING "D"
③ TACK COAT (TC) ITEM 403-02.01 TRACKLESS TACK COAT SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD	④ ASPHALTIC CONCRETE SURFACE (HM) @ 1.25"± THICK (APPROX. 132.5 LBS./S.Y.) ITEM 411-01.07 ACS MIX (PG64-22) GRADING "E" SHOULDER
⑤ COLD PLANING @ 1.25"± THICK (APPROX. 131.3 LBS./S.Y.) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT	⑥ COLD PLANING @ 4"± THICK (APPROX. 420 LBS./S.Y.) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT (TO BE USED FOR BREAKOUT AREAS ONLY)



BRIDGE NOTES				
LOCATION	BRIDGE NO.	LENGTH (FT.)	VERTICAL CLEARENCE OF UNDERPASS	TREATMENT
L.M. 17.07	79I02400115	-	NBL 16' 2" SBL 15' 0"	TO MAINTAIN MINIMUM VERTICAL CLEARENCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT
L.M. 17.58	79I02400117	-	NBL 16' 7" SBL 16' 9"	TO MAINTAIN MINIMUM VERTICAL CLEARENCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT
L.M. 19.18R	79I02400119	120'-10 1/2"		SEAL WITH TYPE 1 THIN EPOXY OVERLAY/REPLACE BRIDGE JOINTS (BRIDGE SHEETS INCLUDED)
L.M. 19.19	N/A	-		TO MAINTAIN MINIMUM VERTICAL CLEARENCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT
L.M. 19.20L	79I02400120	79'-6"		LEAVE AS IS
L.M. 19.40	79I02400136B	-	CL 19' 6"	TO MAINTAIN MINIMUM VERTICAL CLEARENCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT
L.M. 19.40	79I00400167	-	CL 25' 0"	TO MAINTAIN MINIMUM VERTICAL CLEARENCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT
L.M. 19.41L	79I00400102	212'-7/8"		COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW ASPHALT. IF COLD PLANING IS NOT IN THE PROJECT LEAVE BRIDGE AS IS.
L.M. 19.41R	79I00400101	205'-8 1/4"		COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW ASPHALT. IF COLD PLANING IS NOT IN THE PROJECT LEAVE BRIDGE AS IS.
L.M. 19.50	79I00400168B	-	CL 17' 6"	TO MAINTAIN MINIMUM VERTICAL CLEARENCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT
L.M. 19.54	79I00400099	121'-3 1/4"		COLD PLANE 0.5" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW ASPHALT. IF COLD PLANING IS NOT IN THE PROJECT LEAVE BRIDGE AS IS.
L.M. 19.59R	79I00400097	54'		PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 19.59L	79I00400098	52'		PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 19.64R	79I00400095	141'-2"		COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW ASPHALT. IF COLD PLANING IS NOT IN THE PROJECT LEAVE BRIDGE AS IS.
L.M. 19.64L	79I00400096	138'-6"		COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW ASPHALT. IF COLD PLANING IS NOT IN THE PROJECT LEAVE BRIDGE AS IS.

SEALED BY



1/7/2025

NOT TO SCALE

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TYPICAL  
SECTIONS AND  
PAVEMENT  
SCHEDULE



GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

MISCELLANEOUS

- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6” LINE), L.M.

FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
  - a. SHOULDERS SHALL BE BROOMED AND DE-GRASSED AND MATERIAL SHALL BE PICKED UP AND REMOVED. THIS WILL BE PAID FOR UNDER ITEM NO. 208-01.05.
  - b. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (9) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6” ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.
- (10) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 8” ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.03, ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE), L.F. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (20) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

SIGNALIZATION


- (1) EQUIPMENT AND INSTALLATION OF TRAFFIC SIGNALS SHALL COMPLY WITH TDOT STANDARD SPECIFICATIONS, SECTION 730.
- (7) IF RESURFACING IS INCLUDED IN THE PROJECT, SIGNAL DETECTION LOOPS SHALL BE INSTALLED BEFORE THE FINAL SURFACE IS APPLIED.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-240-1(299)	2C

SEALED BY



1/7/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

GENERAL  
NOTES

SPECIAL NOTES

RESURFACING

- (1) SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (8) FEATHER SURFACE MIX TO ENDS OF BRIDGES THAT ARE NOT TO BE PAVED.
- (9) TRAFFIC WILL BE ALLOWED TO TEMPORARILY DRIVE ON THE MILLED SURFACE OF THE ROADWAY UNDER THE FOLLOWING CONDITIONS ONLY:
  - A. THE MILLED SURFACE IS FINE TEXTURED. THE FINE TEXTURE SHALL BE OBTAINED BY A MILLING MACHINE UTILIZING A MILLING HEAD WITH TEETH SPACING 3/8" OR LESS OPERATING AT LESS THAN 80 FEET PER MINUTE.
  - B. THE SURFACE SHALL BE SWEEPED AND CLEANED OF ALL LOOSE MATERIALS.
  - C. THE MILLED SURFACE SHALL BE PAVED WITHIN 72 HOURS IF THE CURRENT ADT IS ≥ 70,000 OR WITHIN 96 HOURS IF THE CURRENT ADT IS < 70,000.
  - D. RAIN OR INCLEMENT WEATHER IS NOT EXPECTED OR FORECASTED WITHIN 48 HOURS AFTER MILLING.
  - E. ALL APPLICABLE SIGNING IS INSTALLED IN ACCORDANCE WITH THE MUTCD. SIGNING SHALL INCLUDE MOTORCYCLE WARNING SIGNS (TN-64) PLACED IN ADVANCE OF ANY MILLED AREAS
  - F. IF MILLED SURFACE BEGINS TO DETERIORATE, PAVING TO COVER UP DETERIORATING MILLED SURFACES SHOULD OCCUR AS DIRECTED BY THE ENGINEER DURING THE NEXT WORKING DAY. IF SEVERE DISTRESS OCCURS, IMMEDIATE RESPONSE WILL BE REQUIRED.
  - G. ONLY ONE LANE IN EACH DIRECTION SHALL HAVE A MILLED SURFACE AT ONE TIME.

PAVEMENT MARKING

- (1) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20
- (2) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-12.02

SIGNS

- (1) IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

MISCELLANEOUS

- (1) ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) THE CONTRACTOR SHALL KEEP TWO TRAFFIC LANES, ONE IN EACH DIRECTION, OPEN TO TRAFFIC DURING NON-WORK HOURS OR NON-WORK DAYS.
- (2) THE DEPARTMENT SHALL RESERVE THE RIGHT TO REOPEN LANES AS NECESSARY WHEN TRAFFIC CONDITIONS ARE DEEMED UNACCEPTABLE (EXCESSIVE QUEUE LENGTHS AND DELAY TIMES). THE CONTRACTOR SHALL BE REQUIRED TO FULLY COOPERATE WITH THE PROJECT SUPERVISOR WHEN REQUESTED TO MAKE CHANGES TO THE TRAFFIC CONTROL.
- (3) MESSAGE BOARDS SHALL BE USED NEAR INTERCHANGES AND/OR OTHER DESIGNATED AREAS IN ADVANCE OF THE WORK ZONE TO ALERT MOTORISTS OF POSSIBLE DELAYS AND RECOMMEND THE USE OF ALTERNATE ROUTES. THE MESSAGES SHALL BE UPDATED AS OFTEN AS NECESSARY SO THAT THE MOTORISTS OBTAIN CURRENT TRAFFIC INFORMATION. MESSAGE BOARDS SHALL BE RELOCATED AS NECESSARY AS THE WORK PROGRESSES. THE CONTRACTOR SHALL BE REQUIRED TO IDENTIFY AN INDIVIDUAL WORKING ON THE PROJECT THAT WILL BE RESPONSIBLE FOR KEEPING THE MESSAGES CURRENT AND RELOCATING MESSAGE BOARDS AS REQUESTED BY TDOT. THE PROJECT SUPERVISOR SHALL HAVE THE AUTHORITY TO APPROVE ALL MESSAGES AND REQUIRED CHANGES AT ANY TIME DUE TO CHANGING TRAFFIC CONDITIONS.

WORK RESTRICTIONS

- (1) CONSTRUCTION ACTIVITIES SHALL NOT INTERFERE WITH SCHOOL OR WORK TRAFFIC EITHER AM OR PM AS DIRECTED BY THE ENGINEER.

JOINT SEALANTS

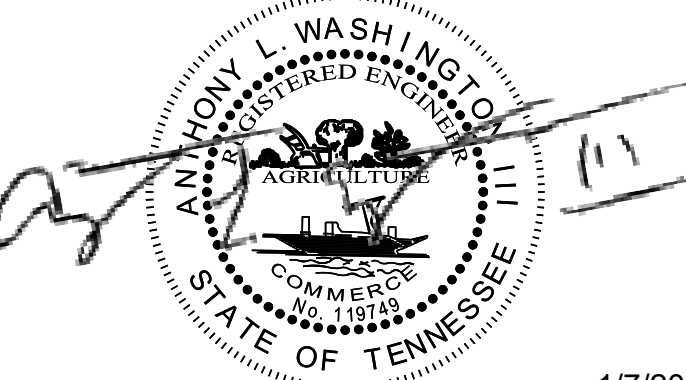
- (1) THE CONTACT SURFACE OF TRANSVERSE JOINTS AND LONGITUDINAL JOINTS IN THE SURFACE LIFT SHALL BE SEALED BY APPLYING JOINT SEALANT PRIOR TO PLACEMENT OF ADDITIONAL ASPHALT AGAINST THE PREVIOUSLY PLACED MATERIAL. MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED IF THE MATERIAL NEEDS TO BE RE-HEATED, AND WHEN PLACING THE THIN, UNIFORM COAT.
- (2) PRIOR TO APPLICATION OF THE SEALANT, THE FACE OF THE JOINT SHALL BE THOROUGHLY DRY AND FREE FROM DUST OR ANY OTHER MATERIAL THAT WOULD PREVENT PROPER SEALING. ALL JOINTS SHALL BE SWEEPED OR BLOWN FREE OF LOOSE MATERIAL, DIRT, VEGETATION, AND OTHER DEBRIS BY MEANS OF COMPRESSED AIR OR A POWER SWEEPER.
- (3) TRUCK AND VEHICLE TRAFFIC SHALL NOT DRIVE ACROSS A SEALED JOINT UNTIL IT HAS DRIED SUFFICIENT TO PREVENT DAMAGE FROM TRACKING.

SPECIAL TRAFFIC COUNTER SPECIFICATIONS

- (1) CONTRACTOR SHALL SUPPLY AND INSTALL ALL RELATED EQUIJPMNT SO THAT ONCE A TRAFFIC COUNTER AND MODEM ARE INSTALLED BY THE STATE'S PLANNING DIVISION'S TRAFFIC COUNTER PERSONNEL, THE COUNT STATION WILL BE FULLY OPERATIONAL. THE CONTRACTOR SHALL MAKE SURE EACH DETECTION LOOP IS OPERATIONAL AT THE COMPLETION OF THE PROJECT.
- (2) COUNTER CABINET SHALL NOT TO BE PRE-WIRED FOR A TRAFFIC SIGNAL AND DOES NOT NEED TO INCLUDE A VENTILATION FAN. THE CABINET SHALL INCLUDE ONE (1) SHELV. THE CABINET SHALL CONTAIN AN AC PLUG FOR TWO (2) RECEPTICALS.
- (3) THE CABINET DOOR SHALL BE SEALED WITH A CLOSED CELL NEOPREME GASKET BONDED TO THE INSIDE OF THE DOOR WITH AN OIL RESISTENT ADHESIVE THAT WILL BE POSITIONED TO PREVENT ANYTHING FROM PENTRATING THROUGH THE CRACK BETWEEN THE DOOR AND THE CABINET WHEN THE DOOR IS CLOSED. THE CABINET SHALL BE MOUNTED SO THAT THE CABINET DOOR WILL OPEN TOWARD THE ROADWAY.
- (4) THE CABINET SHALL BE POLE-MOUNTED TO A FOUR INCH (4") GALVANIZED, RIGID CONDUIT POLE WITH THE BOTTOM OF THE CABINET AT LEAST 36" ABOVE GROUND LEVEL. INSTALLATION SHALL INCLUDE AN EXTERNAL GROUND ROD AT THE BASE OF THE CABINET FOR LIGHTENING PROTECTION.
- (5) A SMALL ELECTRICAL BOX HOUSING A 20 AMP CIRCUIT BREAKER SHALL BE LOCATED ON THE 4" GALVANIZED CONDUIT POLE, BETWEEN THE POWER SOURCE AND CABINET.
- (6) ALL LOOPS SHALL BE FIVE FEET BY SEVEN FEET (5' X 7') AND TYPICAL IN SIZE WITH THREE (3) TURNS OF LOOP WIRE.
- (7) ALL LOOP WIRES SHALL TERMINATE IN THE CABINET CONNECTED TO A TERMINAL STRIP.
- (8) EACH LOOP DETECTOR LEAD-IN SHALL BE MARKED WITH LANE IDENTIFICATION, DENOTING THE LANE AND POSITION IN THE LANE FOR EACH LOOP (SUCH AS NB-1 FOR NORTHBOUND 1, ECT.).
- (9) FOR QUESTIONS CONCERNING ANY OF THE INSTALLATION OF THE COUNTER EQUIPMENT, PLEASE CONTACT STANLEY DUNN AT 615-350-4571. CONTRACTOR SHALL NOTIFY STANLEY DUNN TO REPORT THAT WORK HAS BEEN COMPLETED SO HE CAN FINISH THE INSTALLATION.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-240-1(299)	2D

SEALED BY



1/7/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SPECIAL  
NOTES



ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ENGINEERING PRODUCTION SUPPORT DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

PROJECT COMMITMENTS

- (5) SEE PROJECT COMMITMENTS, SHEET 1B FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

SCOPE OF WORK

- (6) THIS PROJECT INCLUDES COLD PLANING EXISTING ASPHALT, RESURFACING, PAVEMENT MARKINGS, TEMPORARY TRAFFIC CONTROL, AND BROOMING AND DEGRASSING SHOULDERS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

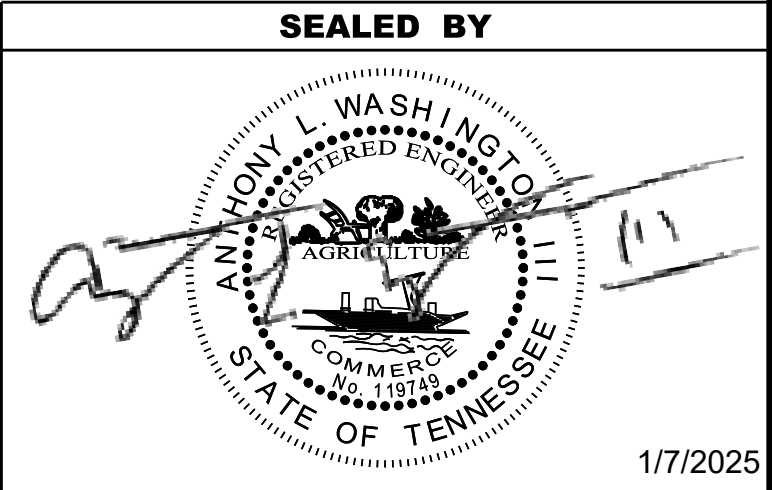
SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-240-1(299)	2E



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL  
NOTES

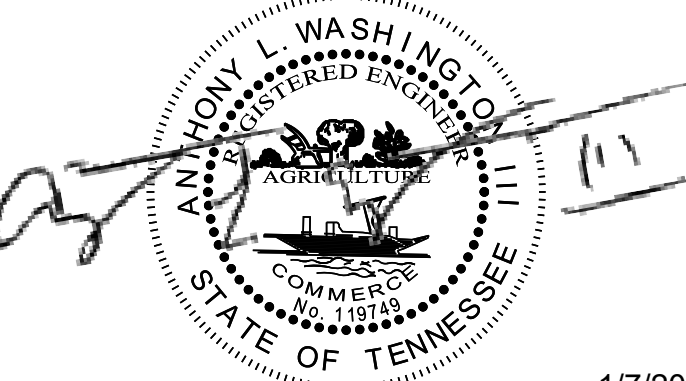
ENVIRONMENTAL NOTES CONT.

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-240-1(299)	2E1

SEALED BY



1/7/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL  
NOTES

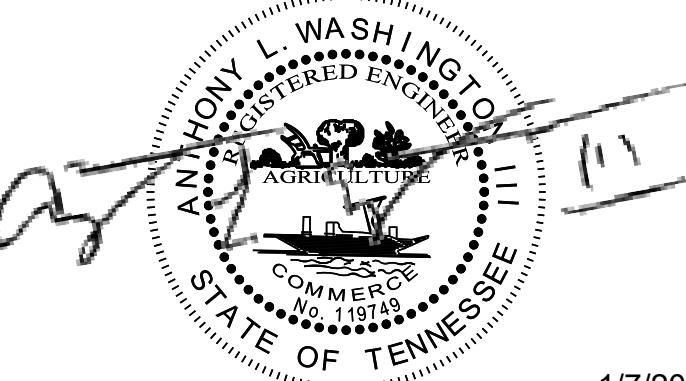


TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-240-1(299)	2F

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)						
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES			TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
		L	x	W		
G20-1	ROAD WORK NEXT 5 MILES	48"	x	24"	8	32
W20-1	ROAD WORK AHEAD	48"	x	48"	16	160
W20-1	ROAD WORK 1 MILE	48"	x	48"	16	64
W20-1	ROAD WORK 1/2 MILE	48"	x	48"	16	64
W20-1	ROAD WORK 1000 FT	48"	x	48"	16	64
W8-11	UNEVEN LANES	48"	x	48"	16	160
W8-15	GROOVED PAVEMENT	48"	x	48"	16	160
W8-15P	MOTORCYCLE (PLAQUE)	30"	x	24"	5	50
G20-2	END ROAD WORK	48"	x	24"	8	80
W21-2	FRESH OIL - PORTABLE	48"	x	48"	16	64
W20-5R	RIGHT LANE CLOSED 1/2 MILE	48"	x	48"	16	64
W20-5R	RIGHT LANE CLOSED 1500 FT	48"	x	48"	16	64
W4-2R	RIGHT LANE ENDS	48"	x	48"	16	64
W4-1R	MERGE RIGHT ARROW	48"	x	48"	16	32
W20-5L	LEFT LANE CLOSED 1/2 MILE	48"	x	48"	16	64
W20-5L	LEFT LANE CLOSED 1500 FT	48"	x	48"	16	64
W20-5R	RIGHT TWO LANES CLOSED 1/2 MILE	48"	x	48"	16	64
W20-5R	RIGHT TWO LANES CLOSED 1500 FT	48"	x	48"	16	64
W20-5L	LEFT TWO LANES CLOSED 1/2 MILE	48"	x	48"	16	64
W20-5L	LEFT TWO LANES CLOSED 1500 FT	48"	x	48"	16	64
W4-2L	LEFT LANE ENDS	48"	x	48"	16	64
W3-2	YIELD AHEAD	48"	x	48"	16	32
R1-2	YIELD AHEAD	48"	x	48"	16	32
E5-2	EXIT OPEN	48"	x	36"	12	12
E5-1	EXIT	72"	x	60"	30	30
W8-17	SHOULDER DROP-OFF	48"	x	48"	16	160
W8-17P	SHOULDER DROP-OFF PLAQUES	36"	x	30"	8	75
THIS CONSTRUCTION SIGNING IS TO BE A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.					TOTAL	1911

RAMP, ACCELERATION LANE, AND DECELERATION LANE TABULATION											
MAINLINE	DIRECTION	LOG MILE (L.M.)	ITEM NO. 403-02.01	ITEM NO. 411-03.10	ITEM NO. 415-01.01	ITEM NO. 716-01.23	ITEM NO. 716-01.30	ITEM NO. 716-02.23	ITEM NO. 716-12.02	ITEM NO. 716-12.03	ITEM NO. 716-12.05
			TON	TON	TON	EACH	EACH	L.F.	L.M.	L.F.	L.F.
I-240 ENTRANCE RAMP	EB	16.30	3.3	514.0	481.4	50.0	50.0		1.0	1778.0	476.0
I-240 EXIT RAMP	EB	17.32	2.2	343.0	321.6	30.0	30.0		0.7	1117.0	447.0
I-240 ENTRANCE RAMP	EB	17.77	1.0	155.0	145.2	13.0	13.0		0.5	472.0	
I-240 EXIT RAMP	EB	19.16	1.4	226.0	212.1	60.0	60.0	1452.0	2.3		2606.0
I-240 ENTRANCE RAMP	EB	19.39	1.6	253.0	237.2	10.0	10.0		0.6	423.0	
I-240 EXIT RAMP	EB	19.48	6.0	940.0	881.5	25.0	25.0	1007.0	0.8		344.0
I-240 EXIT RAMP	EB	19.71	0.4	60.0	55.8	10.0	10.0		0.5	200.0	85.0
I-240 ENTRANCE RAMP	EB	19.85	0.8	130.0	121.5	10.0	10.0		0.5	429.0	
I-240 ENTRANCE RAMP	EB	19.98				50.0	50.0		2.0	3017.0	336.0
I-240 EXIT RAMP	WB	16.26	3.1	490.0	459.2	50.0	50.0		0.9	1408.0	317.0
I-240 ENTRANCE RAMP	WB	17.00	8.1	1259.8	1181.0	50.0	50.0		2.5	493.0	1921.0
I-240 EXIT RAMP	WB	17.78	5.4	845.0	791.6	25.0	25.0	2179.0	1.7		421.0
I-240 ENTRANCE RAMP	WB	19.29	0.5	74.0	69.2	25.0	25.0		0.6	372.0	1305.0
I-240 EXIT RAMP	WB	19.64	2.3	342.0	341.3	40.0	40.0	1012.0	0.8		1629.0
I-240 ENTRANCE RAMP	WB	19.98	4.2	650.0	610.1	10.0	10.0		2.2	2977.0	
TOTAL			40.0	6282.0	5908.0	458.0	458.0	5650.0	18.0	12686.0	9887.0

SEALED BY



11

1/7/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TABULATED  
QUANTITIES

UTILITY NOTES

UTILITY

- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED ‘AROUND’ UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR’S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

WATER, GAS, ELECTRIC:

MLG&W

220 South Main St.  
Memphis, Tn. 38103  
CONTACT: Darryl McLemore  
OFFICE PHONE: 901 528 4186  
Email: [dmclemore@mlgw.org](mailto:dmclemore@mlgw.org)

TELEPHONE:

AT&T

315 E. College Street  
Jackson, TN. 38301  
CONTACT: Daniel R. Potts  
OFFICE PHONE: 901 488 2359  
Email: [dp7607@att.com](mailto:dp7607@att.com)

COMMUNICATIONS:

Comcast

5450 Winchester Road  
Memphis, Tn. 38115  
CONTACT: Andrew Smith  
OFFICE PHONE: 901 623 7471  
CELL PHONE: 901 208 6380  
Email: [andrew\\_smith6@comcast.com](mailto:andrew_smith6@comcast.com)

FIBER OPTIC:

Centurylink (Qwest) / Lumen (Level 3)

8110 Cordova Rd. Ste. 101  
Cordova, Tn. 38016  
CONTACT: Brian McGREGOR  
OFFICE PHONE: 901 435 2025  
Email: [brian.mcgregor@lumen.com](mailto:brian.mcgregor@lumen.com)

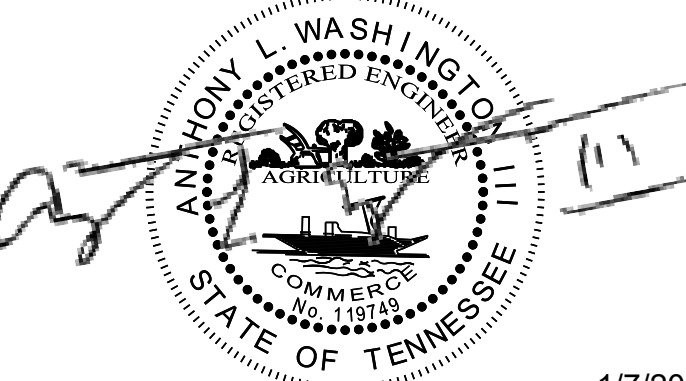
COMMUNICATIONS:

Verizon / XO Communications

5127 Truse Rd  
Memphis, TN 38117  
CONTACT: Robert Stafford  
OFFICE PHONE: 901 239 2912  
Email: [robert.stafford@verizon.com](mailto:robert.stafford@verizon.com)

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-240-1(299)	3

SEALED BY



1/7/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

UTILITY NOTES  
AND UTILITY OWNERS



PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

a.

WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b.

IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

a.

THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
- IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.
- b.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.

c.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.

d.

THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.
- FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.
4.

FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.
- SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.
- IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.
- B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:
1.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.

a.

WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

2.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

3.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b.

ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1.

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.


2.

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH-I-240-1(299)	4

SEALED BY



1/7/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE  
DROP-OFF NOTES  
FOR  
TRAFFIC CONTROL

1/7/2025 1:15:11 PM C:\PWORKING\EA\ST01\ID343533\02D.DGN







FED. PROJ. NO. NH-I-240-1(299)

REVISIONS[illegible]

DWG. NO.

SIGNATURE SHEET	STRUCTURE-SIGN 1
INDEX OF DRAWINGS	
BRIDGE TABULATION, ESTIMATED QUANTITIES, AND EXPANSION JOINT REPAIR NOTES	B1
APPROACH SLAB REPAIR DETAIL NOTES AND TRAFFIC CONTROL QUANTITIES	B2
BRIDGE DECK REPAIR NOTES	B3
TYPE 1 THIN EPOXY OVERLAY NOTES	B4
PLAN VIEW (79102400119)	B5
PHASE CONSTRUCTION	B6
	B7

DWG. NO.

LAYOUT OF BRIDGE \_\_\_\_\_ U-63-120  
SUPERSTRUCTURE \_\_\_\_\_ U-63-124

12/23/24

STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION

## INDEX OF DRAWINGS

79-I240-19.18R

OVER

## OVER GREENWAY

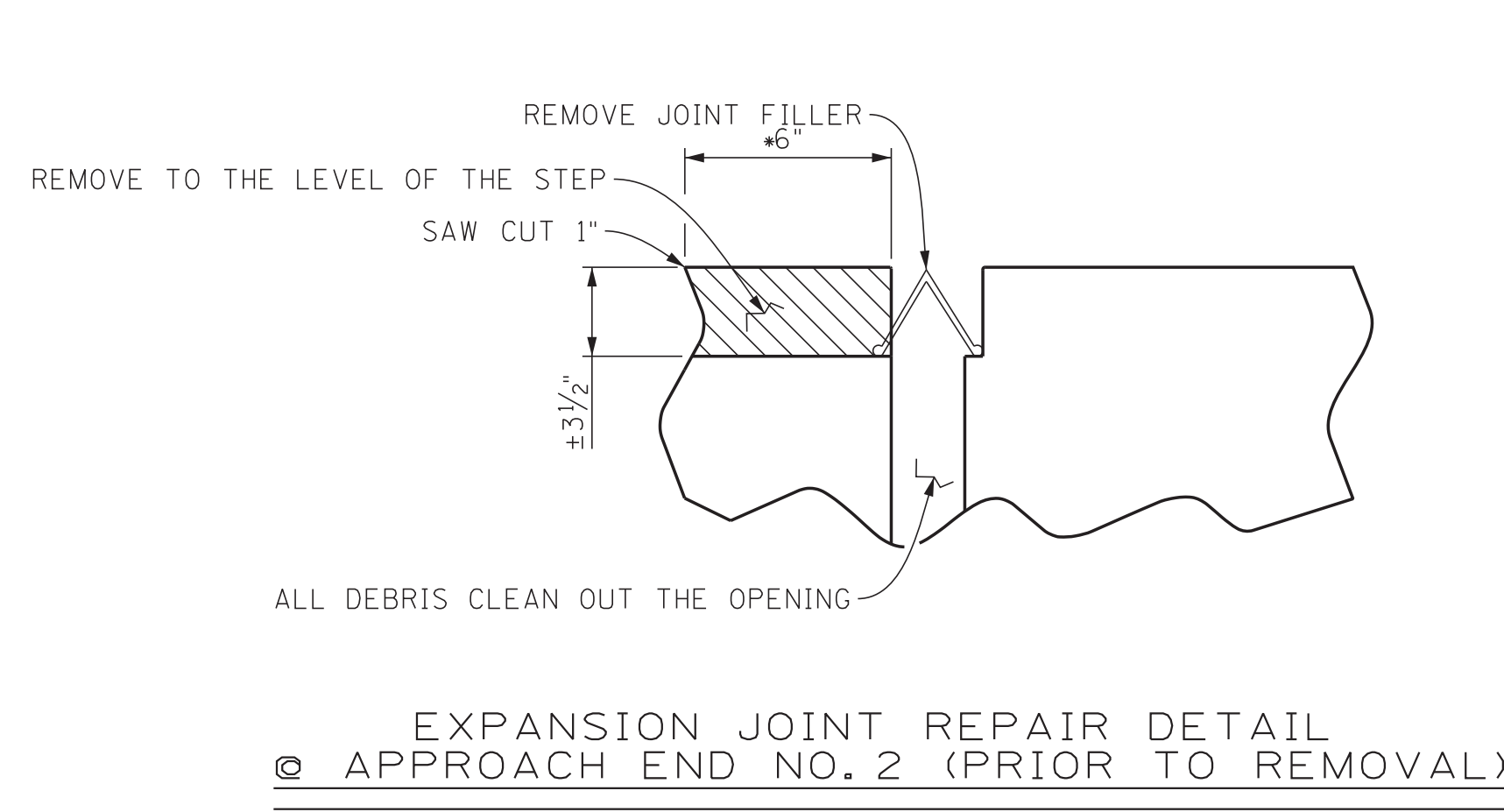
R. NOS. 79I024001

BY COI

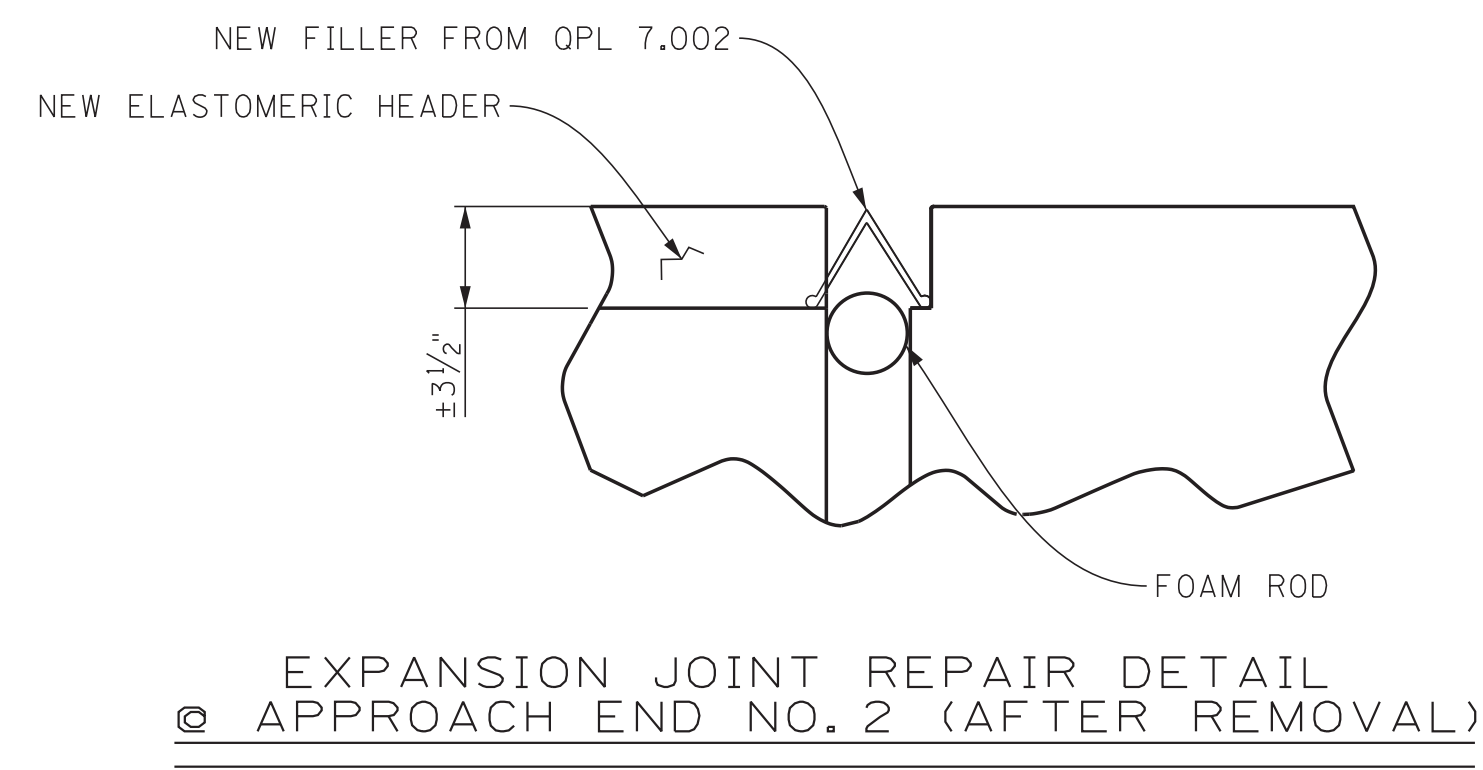
B1

PIN 132479.00

# EXPANSION JOINT REPAIR NOTES



THE COST FOR REMOVING THE OLD JOINT SYSTEM, INSTALLING THE NEW JOINT SYSTEM, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO INSTALL THE NEW EXPANSION JOINT, IS TO BE INCLUDED UNDER ITEM NUMBER 604-10.44, EXPANSION JOINT REPAIRS, L.F.



BRIDGE TABULATION,  
ESTIMATED QUANTITIES, AND  
EXPANSION JOINT REPAIR  
NOTES  
79-1240-19.18R  
OVER  
GREENWAY  
BR. NOS. 79I02400119  
SHELBY COUNTY  
2025

SYTIME 12:00:30 PM



NOTE: REMOVE CONCRETE TO A DEPTH OF  $\frac{3}{4}$ " MINIMUM BELOW EXISTING REINFORCING STEEL OR TO SOUND CONCRETE. CONCRETE REMOVAL SHOULD BE TO A MINIMUM DEPTH OF 4". DEPTH FOR CONCRETE REMOVAL SHOULD NOT BE MORE THAN 6".

NOTE: COST OF SAW CUTTING, CONCRETE REMOVAL, COMPLETELY CLEANING EXPOSED REINFORCING STEEL, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN ITEM NO. 604-10.53 CONCRETE REPAIR (S.V.).

NOTE: ITEM NO. 604-10.53, CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT).  
THIS MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED  
BY THE ENGINEER.

POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE  
IN MAKING PARTIAL DEPTH REPAIRS ARE SUBJECT TO THE  
FOLLOWING RESTRICTIONS:

(1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS  
HEAVIER THAN NOMINAL 60 LB. CLASS SHALL NOT BE USED. TRAFFIC  
CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC.

(2) CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

ITEM 712-06 SIGNS (CONSTRUCTION)

DATE \_\_\_\_\_  
DATE 11/24  
DATE 11/24  
DATE \_\_\_\_\_

REPAIRS SHALL USE AN EXTENDED NON-MAGNESIUM PHOSPHATE PRODUCT FROM THE TDOT QUALIFIED PRODUCTS LIST:  
13.004-RAPID SET CEMENTITIOUS PATCHING MATERIALS.  
MATERIAL SHALL BE APPLIED ACCORDING TO MANUFACTURER'S SPECIFICATIONS.

MIX MUST MEET 3000 PSI BEFORE OPENING TO TRAFFIC.

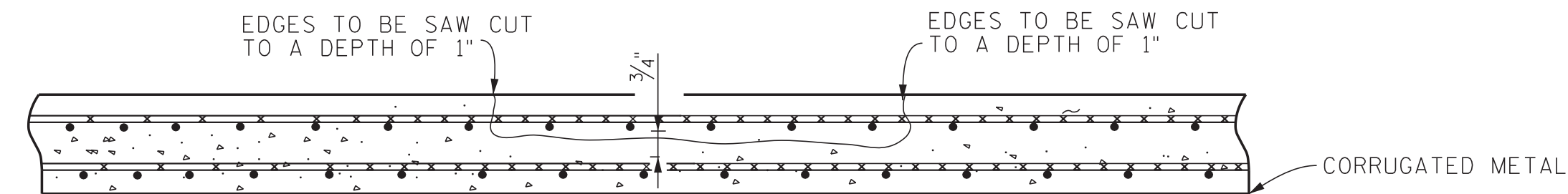
B3



PIN 132479.00

[illegible]

USE AN EXTENDED RAPID SET CEMENTITIOUS  
PATCHING MATERIALS (FOR PATCHING MATERIAL  
REFER TDOT QUALIFIED PRODUCT LIST NO.13.004)



SKETCH SHOWING DECK REPAIR (STRUCTURAL REPAIR):

NOTE: PARTIAL DEPTH DECK REPAIR ONLY REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF  $\frac{3}{4}$ " BELOW THE BOTTOM BAR OF THE TOP MAT OF REINFORCING STEEL.

NOTE: PARTIAL DEPTH DECK REPAIRS:  
ALL REINFORCING STEEL IN THE AREAS OF THE DECK REPAIRS SHALL BE COMPLETELY CLEANED.  
CLEANING SHALL BE DONE PRIOR TO PLACING NEW CONCRETE. AREAS OF CONCRETE REMOVAL  
SHALL BE DESIGNATED BY PERSONNEL FROM THE HEADQUARTERS, BRIDGE INSPECTION AND REPAIR  
OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE  
BRIDGE REPAIR OFFICE AT LEAST (3) DAYS IN ADVANCE.

THE COST OF CONCRETE, CONCRETE REMOVAL, LABOR, CLEANING REBARS AND ANY MISCELLANEOUS MATERIAL REQUIRED SHALL BE PAID FOR UNDER ITEM NO. 604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB), S.Y.

NOTE: ITEM NO. 604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB)  
MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

POWER DRIVEN HAND TOOLS:

POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

(1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS  
HEAVIER THAN NOMINAL 60 lb. CLASS SHALL NOT BE USED. TRAFFIC  
CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC.

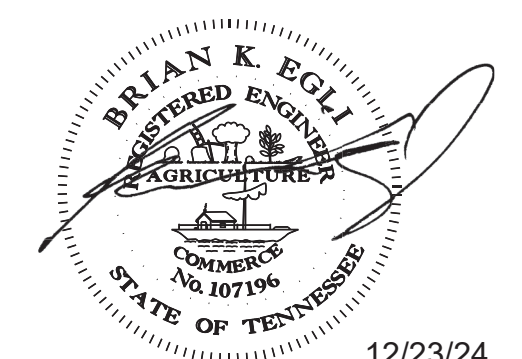
(2) CHIPPING HAMMERS OF THE 15 lb. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

## BRIDGE DECK REPAIR MATERIAL:

REPAIRS SHALL USE AN EXTENDED NON-MAGNESIUM PHOSPHATE PRODUCT FROM THE TDOT QUALIFIED PRODUCTS LIST:  
13.004-RAPID SET CEMENTITIOUS PATCHING MATERIALS.  
MATERIAL SHALL BE APPLIED ACCORDING TO MANUFACTURER'S SPECIFICATIONS.  
MIX MUST MEET 3000 PSI BEFORE OPENING TO TRAFFIC.

[illegible]

DESIGNED BY \_\_\_\_\_ DATE \_\_\_\_\_  
 DRAWN BY Z.HAYNES DATE 11/24  
 SUPERVISED BY K. MARTINKO DATE 11/24  
 CHECKED BY \_\_\_\_\_ DATE \_\_\_\_\_



12/23/24

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

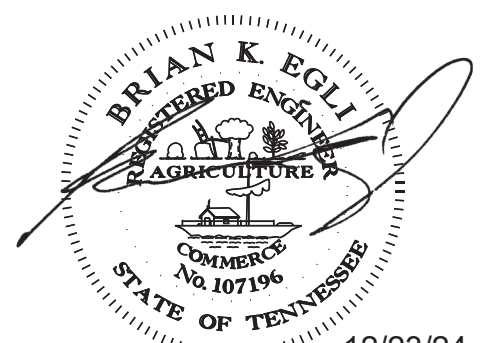
BRIDGE DECK  
REPAIR NOTES  
79-1240-19.18R  
OVER  
GREENWAY  
BR. NOS. 79I02400119  
SHELBY COUNTY  
2025



[illegible]


 NOTE: ALL PARTIAL REPAIR AREAS SHALL HAVE AN EPOXY OVERLAY AFTER REPAIRED.

DESIGNED BY \_\_\_\_\_ DATE \_\_\_\_\_  
DRAWN BY Z.HAYNES DATE 11/24  
SUPERVISED BY K. MARTINKO DATE 11/24  
CHECKED BY \_\_\_\_\_ DATE \_\_\_\_\_



PLAN VIEW  
79-1240-19.18R  
OVER  
GREENWAY  
BR. NOS. 79102400119  
SHELBY COUNTY  
2025



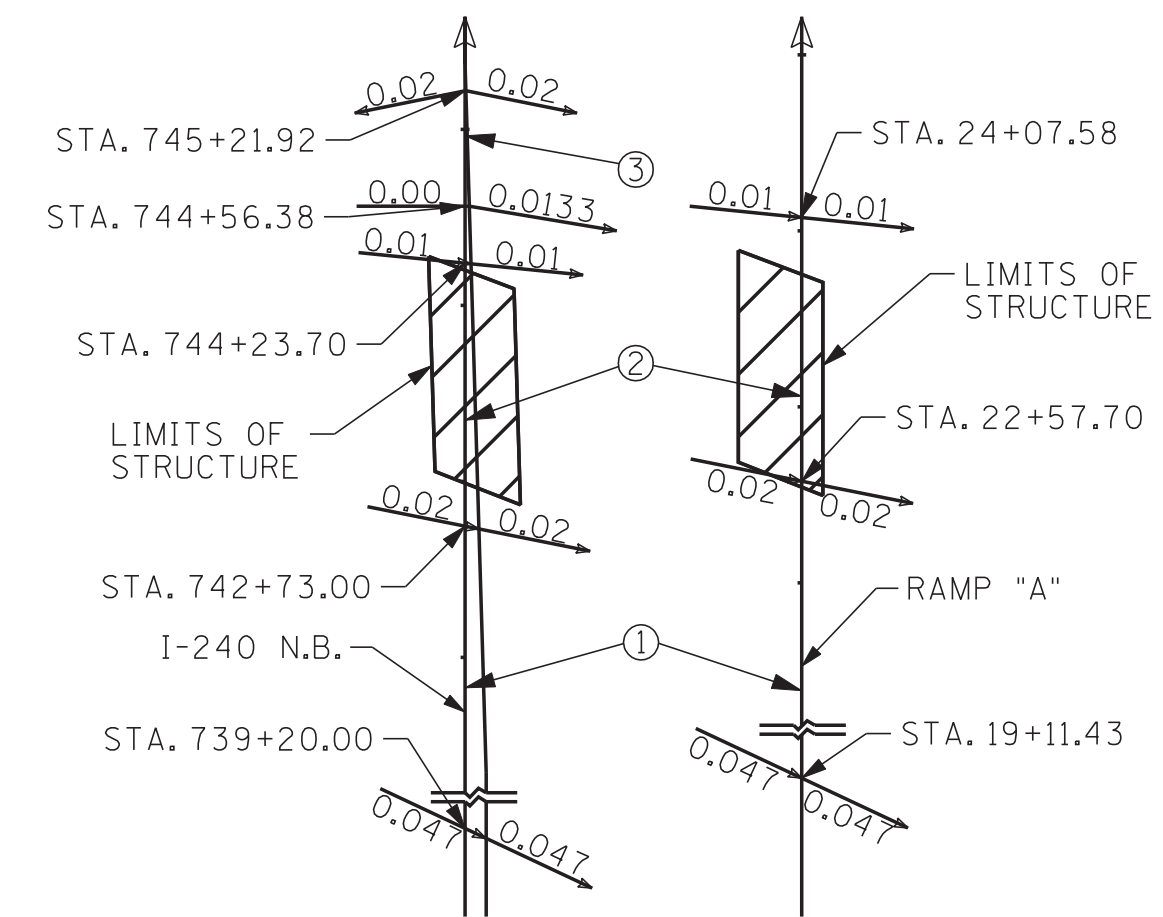
PROJECT NO.	YEAR	SHEET NO.
791240-M3-009	2025	B7

[illegible]

■ DENOTES: INTERCONNECTED PORTABLE BARRIER RAIL



B7

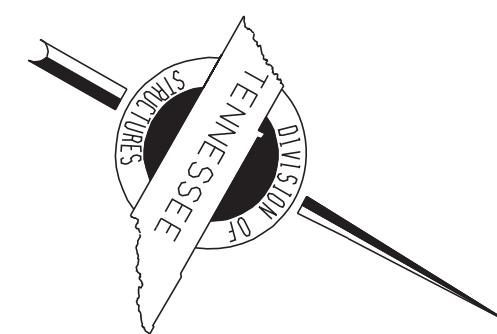


S.E. TRANSITION SKETCH  
(I-240 N.B. & RAMP "A")

```

CURVE DATA
(RAMP A - CURVE 2)
PI 26+46.02
N 320.549.8900
E 808.377.5400
S -----
θS -----
C 37° 14' 04.81" (RT)
Dc 5' 59" 59.13"
Rc 954.97
Lc 620.60
Ts -----
Ls -----
SE 0.056 FT/FT
DESIGN SPEED 40 MPH
TRANS. LENGTH VARIES

```



TRANSITION LENGTH	
	I-240 N.B. RAMP 'A'
①	353' 346.27'
②	150.7' 149.88'
③	98.22' ---

$VPI\ 742.29+0.05$   
 $EL.\ 283.25$

$0.02\%$

$VPC\ 743.34+09.07$   
 $EL.\ 283.27$

$VPI\ 743.34+07$   
 $EL.\ 283.27$

$VPI\ 743.34+59.07$   
 $EL.\ 283.17$

$-0.40\%$

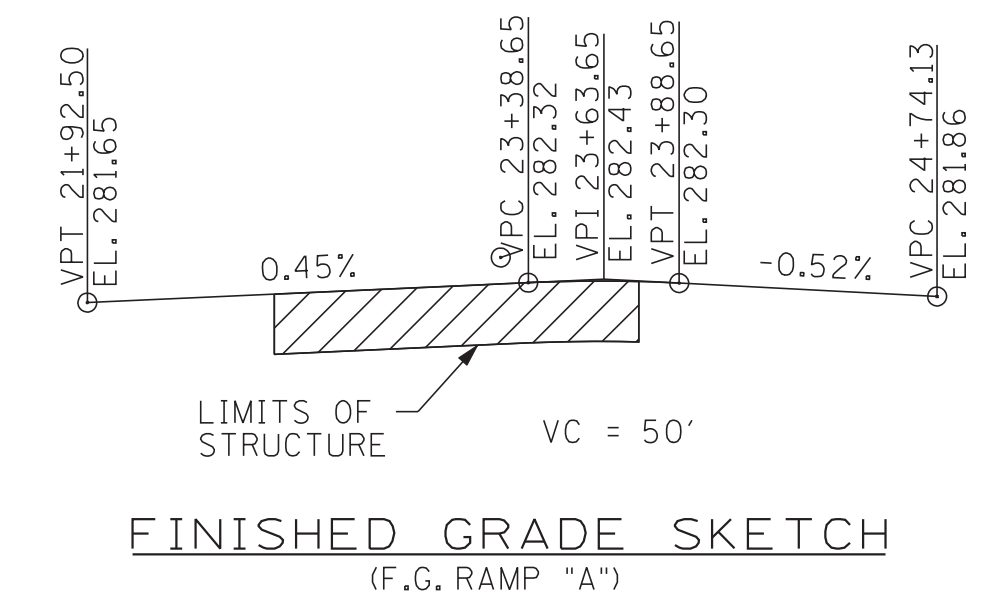
$VPI\ 745.00+00.00$   
 $EL.\ 282.61$

$VC = 50'$

LIMITS OF STRUCTURE

**FINISHED GRADE SKETCH**

(F.G. I-240 N.B.)



LAYOUT OF BRIDGE  
WIDENING OF I-240 N.B.  
€ RAMP "A" OVER GREENWAY  
BRIDGE I.D. NO. 79I02400119  
I-240 N.B. STATION 743+64.03  
LOG MILE 19.04  
SHELBY COUNTY  
2013



CORRECT Wayne J. Seger  
ENGINEER OF STRUCTURES

U-63-120

CLASS A-3 RIP-RAP = 290 TONS





NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

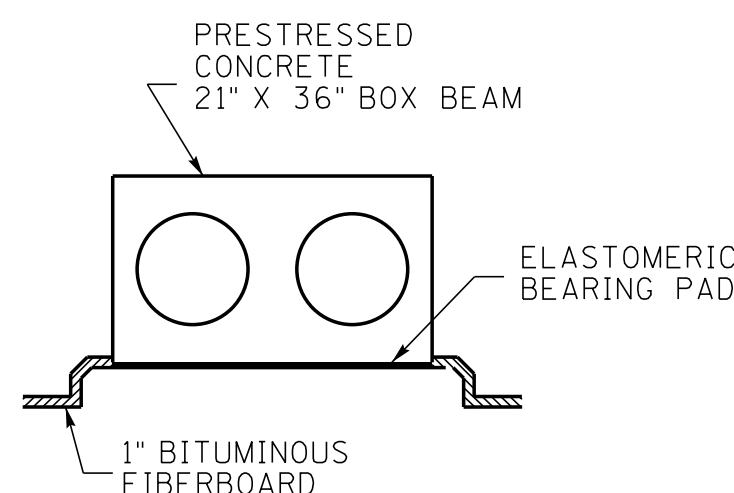
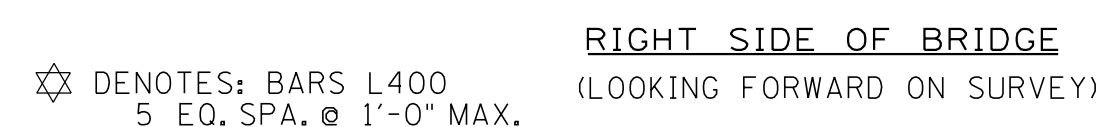
NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO, SEE DRAWING NO. STD-1-ISS.

NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION.

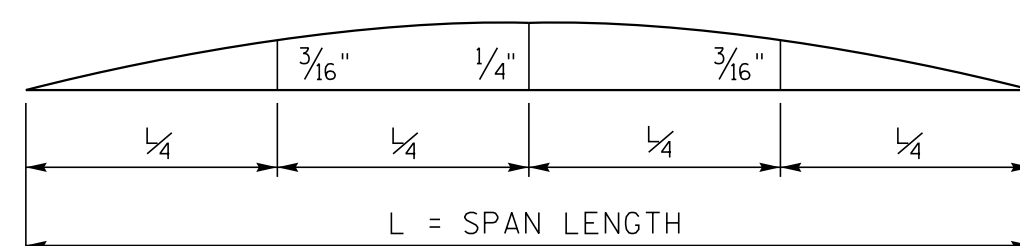
NOTE: THE SUPPORT DIAPHRAGMS AT THE BENTS SHALL BE FORMED AND THE BOTTOM 15 INCHES POURED AS SOON AS POSSIBLE AFTER THE BEAMS HAVE BEEN SET. THE REMAINDER OF THE DIAPHRAGM SHALL BE POURED CONCURRENTLY WITH THE DECK SLAB. ALL DIAPHRAGM CONCRETE SHALL BE INCLUDED IN THE QUANTITY FOR ITEM NO. 604-03.09.

NOTE: CONTRACTOR SHALL OFF-SET ANY REINFORCEMENT NECESSARY TO MAINTAIN 2" CLEARANCE TO PHASE CONSTRUCTION JOINT.

BRIDGE DECK SEALANT: REFER TO GENERAL NOTES SHEET U-65-121.



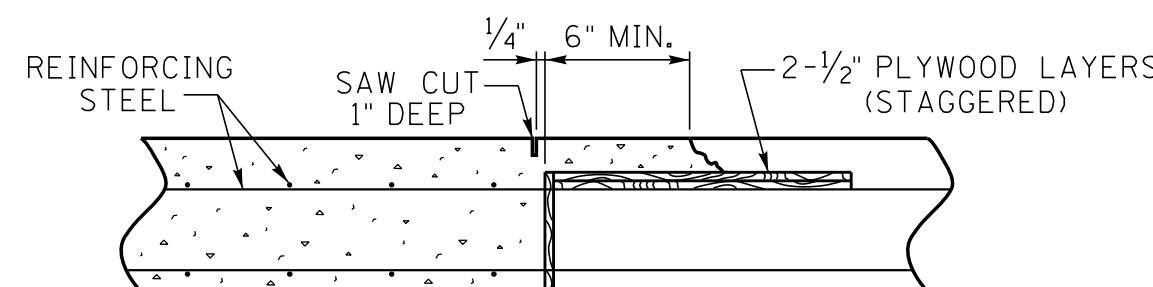
DETAIL "X"



DEAD LOAD CORRECTION CURVE  
(TYP. IN PHASE I)

DEAD LOAD CORRECTION CURVE: THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE AFTER SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE.

NOTE: IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.

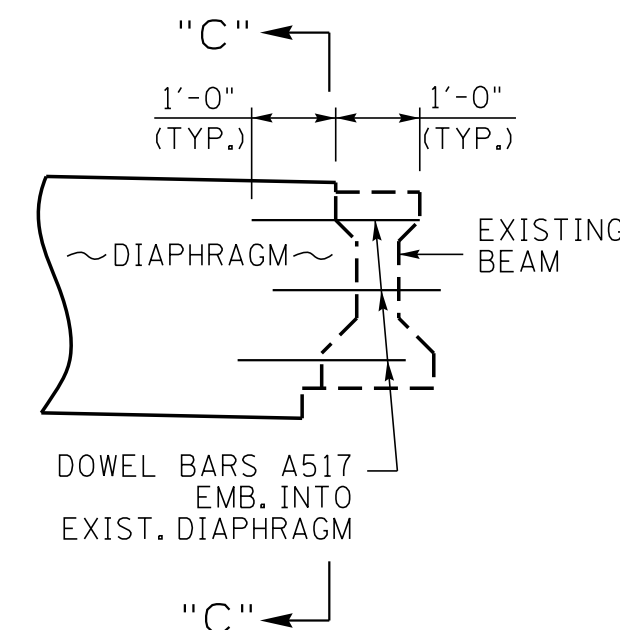


### SLAB CONSTRUCTION JOINT DETAIL

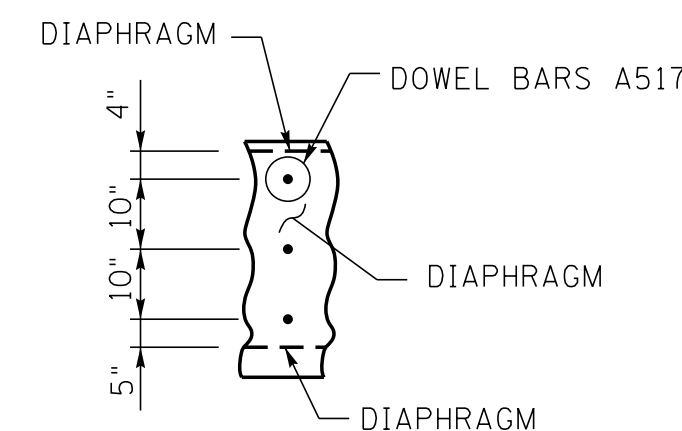
DECK CONCRETE POURING SEQUENCE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING:

1. NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 10 FEET OR FURTHER THAN 15 FEET FROM AN INTERIOR SUPPORT.
2. THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED WITHIN AT LEAST 15 FEET OF THE SUPPORTS EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.

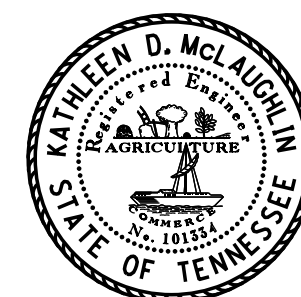
ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE  
SLAB CONSTRUCTION JOINT DETAIL SHOWN ABOVE.



DETAIL "K"



SECTION "C"-"C"



CORRECT Wayne J. Seger  
ENGINEER OF STRUCTURES

BRIDGE NO. 3  
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
SUPERSTRUCTURE  
WIDENING OF I-240 N.B.  
↻ RAMP "A"  
OVER  
GREENWAY  
240 N.B. STATION 743+64.03  
LOG MILE 19.04  
SHELBY COUNTY  
2013